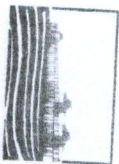


# **Fifth Avenue South / U.S. 41 Urban Design Charrette**



City of Naples Planning Department  
Planning Advisory Board

February 2004



School of Architecture and Community Design

5<sup>th</sup> AVENUE SOUTH/U.S. 41 URBAN DESIGN CHARRETTE  
NAPLES, FL

Summary Report

CITY OF NAPLES PLANNING DEPARTMENT  
PLANNING ADVISORY BOARD

SUMMARY REPORT

February 2004

University of South Florida  
School of Architecture and Community Design  
Florida Center For Community Design and Research

5th Avenue South / U.S. 41 Urban Design Charrette Naples, FL	City of Naples Planning Department Planning Advisory Board	University of South Florida School of Architecture and Community Design	2
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# Summary Report

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## SUMMARY REPORT

February 2004

University of South Florida  
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### Project Team:

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5th Avenue South / U.S. 41 Urban Design Charrette  
Naples, FL

City of Naples Planning Department  
Planning Advisory Board

University of South Florida  
School of Architecture and Community Design



5<sup>th</sup> AVENUE SOUTH/U.S. 41 URBAN DESIGN CHARRETTE  
NAPLES, FL

SUMMARY REPORT

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# PROJECT AREA

The project area for the 5<sup>th</sup> Avenue South/U.S. 41 Urban Design Charrette encompassed a one linear mile section of this corridor as it extends east from the "Four Corners" intersection to the city limits. The primary focus area for this project included the frontages along the north and south sides of the street, as well as the bridge sections over the Gordon River. The charrette also incorporated the "Gateway Triangle" area in the unincorporated county, just east of the U.S. 41/Davis Boulevard/Sandpiper St. intersection. (Project boundaries for the charrette were not defined to incorporate a specific amount of land, only the linear segment of the 5<sup>th</sup> Avenue South/U.S. 41 corridor).

Secondary project areas included existing conditions and development projects directly adjacent to those along 5<sup>th</sup> Avenue South/U.S. 41. Other secondary areas included connections from these areas along existing streets, pedestrian linkages and limited areas identified for waterfront access.





## 2.0 Project Objectives

### PROJECT OBJECTIVES

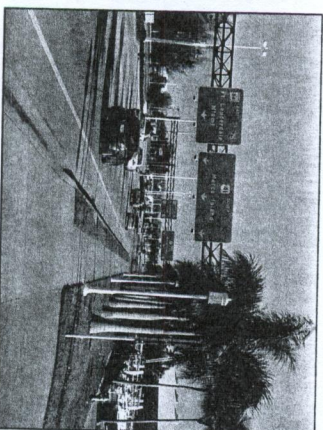
The initial impetus for the 5<sup>th</sup> Avenue South / U.S. 41 Urban Design Charrette was derived from concerns related to future growth and development of Naples' Downtown area. As an extension of the 5<sup>th</sup> Avenue Shopping District on the west and a major entrance into the City from the east, this one-mile section of U.S. 41 will likely realize significant physical change in the next decade. Even though there are a limited number of available development parcels in the immediate area, this part of the City will likely become a new growth corridor for Naples. Another factor that can be attributed to this effort is the number of new projects that are planned for this area and the perceived benefits and impacts they will introduce.

In addition to the economic development, urban design and transportation-related implications of these issues, the Planning Department and the Planning Advisory Board identified the following objectives for the charrette:

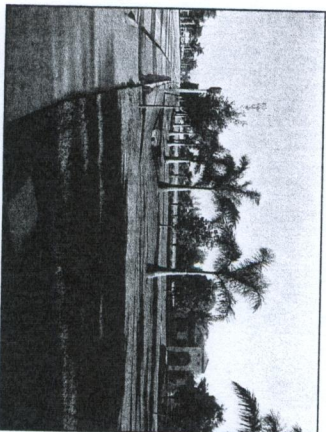
- Solicit input from the general public, property/business owners and the development community about the future of this area.
- Identify opportunities to foster greater pedestrian life, improve visual quality and realize public realm enhancements throughout the area.
- Identify opportunities for economic development and an integrated extension of downtown.
- Identify urban design guidelines that will establish a framework for better coordination of future growth and development.
- Identify aesthetic enhancements to better define entrance conditions into the City from the east.
- Identify issues and problems associated new developments in this area.



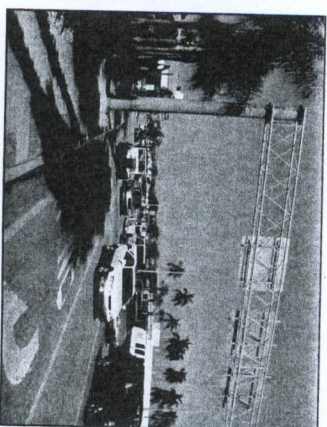
5th Avenue South / U.S. 41 Looking East from Four Corners Intersection



5th Avenue South / U.S. 41 Looking East at Boat Haven Site



Ruffina Development Parcel



5th Avenue South / U.S. 41 Looking North from Four Corners

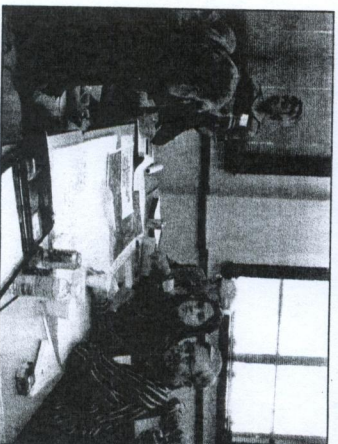
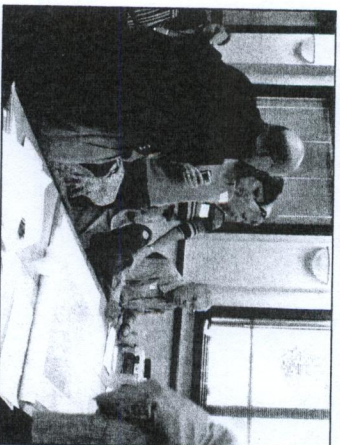
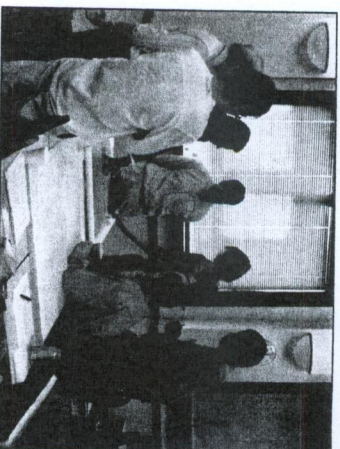
### Existing Conditions



## 3.0 Overview of Charrette Process

### OVERVIEW OF CHARETTE PROCESS

The 5<sup>th</sup> Avenue South/U.S. 41 Urban Design Charrette was conducted over a two-day timeframe. It was held at the Professional Development Center at Gulfview Middle School on Friday and Saturday, January 9 – 10, 2004. Prior to the charrette, the Project Team conducted an informational survey, held discussions with City staff, and conducted a survey of the project area. As a "participatory urban design process", a number of local residents, business/property owners, City staff, members of the Planning Advisory Board, and other professionals attended the charrette. This effort provided a forum for all participants to voice a number of diverse concerns and issues. It also gave participants an active role in assessing current conditions, identifying needs and shortcomings, as well as an opportunity to help craft a shared vision for the future of this area.



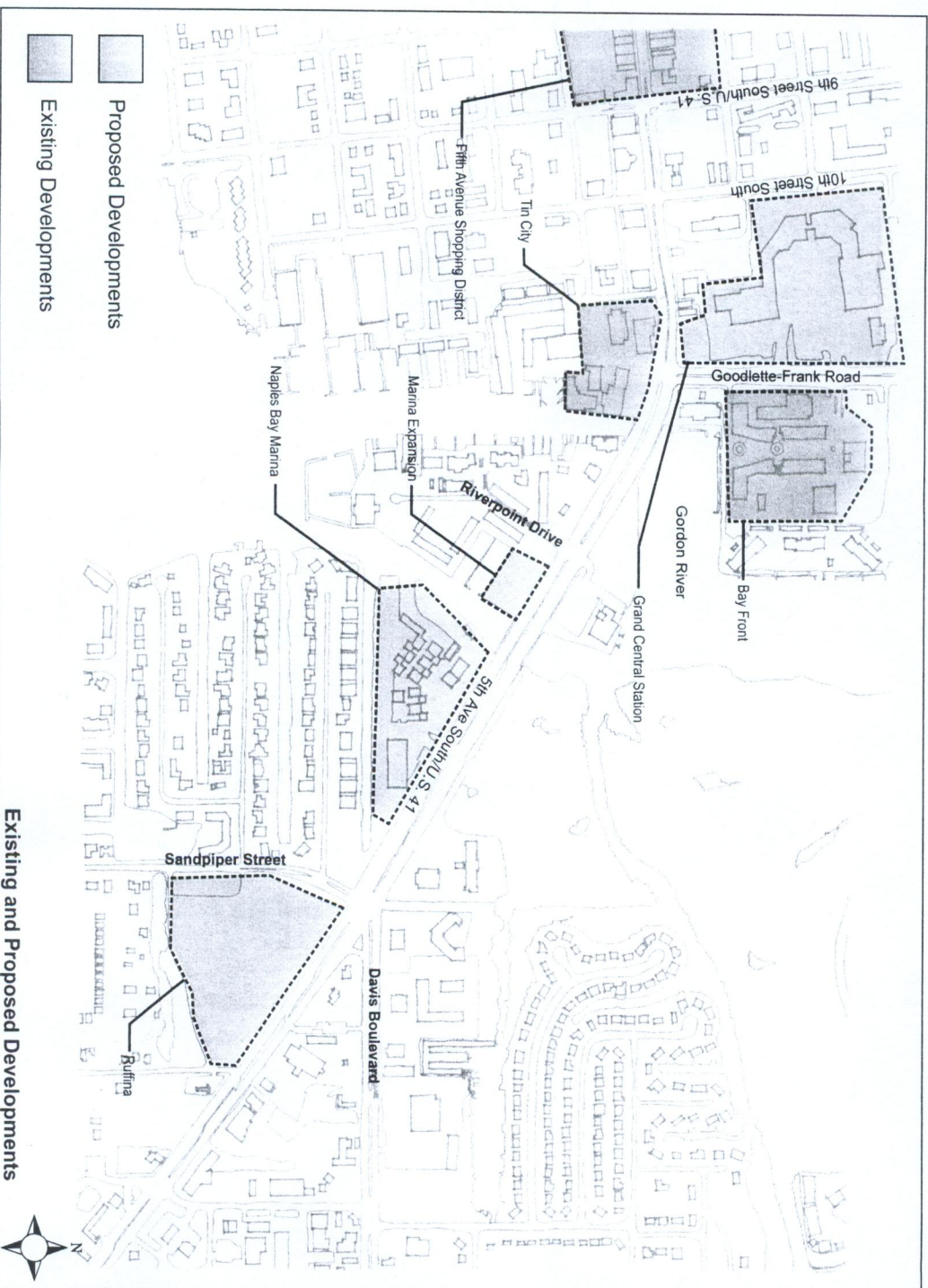


## PROPOSED PROJECTS

Currently there are three significant projects proposed for this portion of the 5<sup>th</sup> Avenue South/ U.S. 41 corridor, as well as an expanded marina just east of Riverpoint Drive. They are:

- Grand Central Station project: a mixed-use development with residential, commercial retail and office uses, with frontage along the northern edge of the corridor, between 10<sup>th</sup> Street and Goodlette-Frank Road.
- Naples Bay Marina: a mixed-use development with a resort hotel, residential and marine-related uses, on the existing Boat Haven site.
- The Ruffina project: a mixed-use development with residential and recreational uses, and a park.
- Marina expansion: a multi-slip marina carved out of an existing landmass.

The Grand Central Station and Naples Bay Marina projects are both well-designed complexes with architectural character that is aesthetically appropriate, provisions for public access and amenity features. All combined, these projects will have a significant impact on this area as they come online. Along with the recent commercial building on the southeast portion of the "Four Corners" intersection and the Bayfront project, these new developments will transform this portion of 5<sup>th</sup> Avenue South into an active commercial corridor in the near-term future. As mentioned earlier, one of the major issues facing the City as these new developments come online will be the extent to which they add to the urban design quality and the unique "sense of place" in the existing Shopping District, and their potential for achieving a high level of integration into the surrounding urban fabric, rather than functioning as isolated projects.





## 5.0 Existing Conditions Analysis

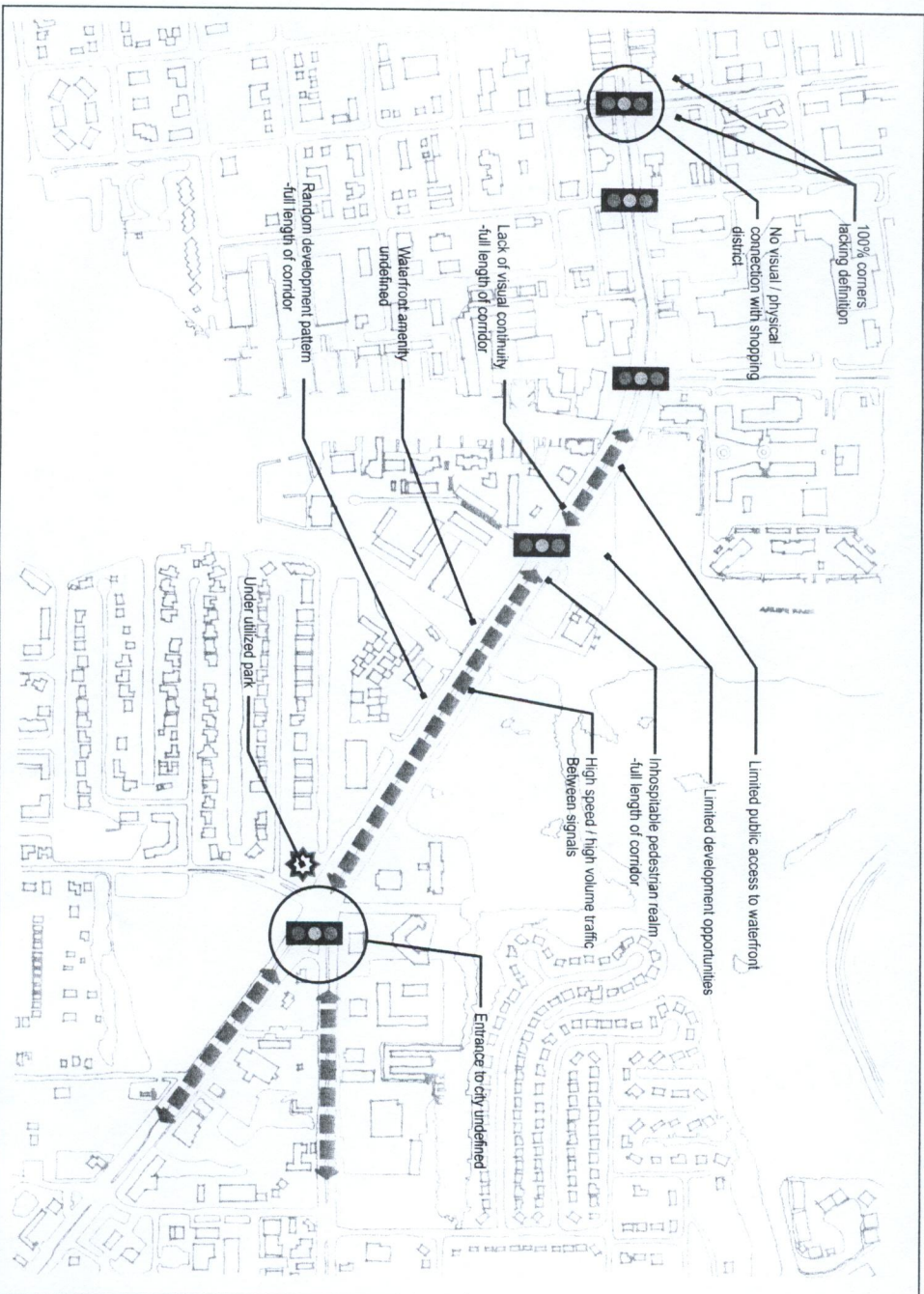
### EXISTING CONDITIONS ANALYSIS

The development pattern, building usage and character and other physical conditions varies widely along the 5<sup>th</sup> Avenue South/U.S. 41 corridor. As mentioned earlier, this part of 5<sup>th</sup> Avenue South bears little resemblance to conditions in the Shopping District. The most striking aspect of this part of the corridor are the "strip-like" or fragmented conditions that have emerged along the broad right of way, in conjunction with a high-volume of local and commuter traffic.

At the western end of the corridor, the project area incorporates the "Four Corners" intersection and the start of the Shopping District – however, only the southeast and southwest corners are defined. The northeast corner, which is the most prominent at this location, is a vacant lot with empty buildings fronting the street. The northwest corner has a somewhat nondescript structure housing the Visitor's Center. Both these corners do not provide visual connections with the Shopping District and could be a significant benefit to the area if they were redeveloped.

Moving east, the Naples Depot, along with several small commercial structures, defines the north part of the street. This area could contribute to the enhancement of the corridor if the Depot property is enhanced and the other frontage is redeveloped. Several commercial buildings that could benefit from exterior renovations and additions occupy the south side of the street. The Tin City development at the end of Goodlette-Frank Road provides a visual terminus to this street as it intersects with 5th Avenue South/U.S. 41. Although it stands striking contrast to any other structures in the immediate area, its viability and unique character may prove to be a long-term asset to this part of the City.

The marine-related uses along the mid-section of the corridor, which are in close proximity to the



Existing Conditions Analysis



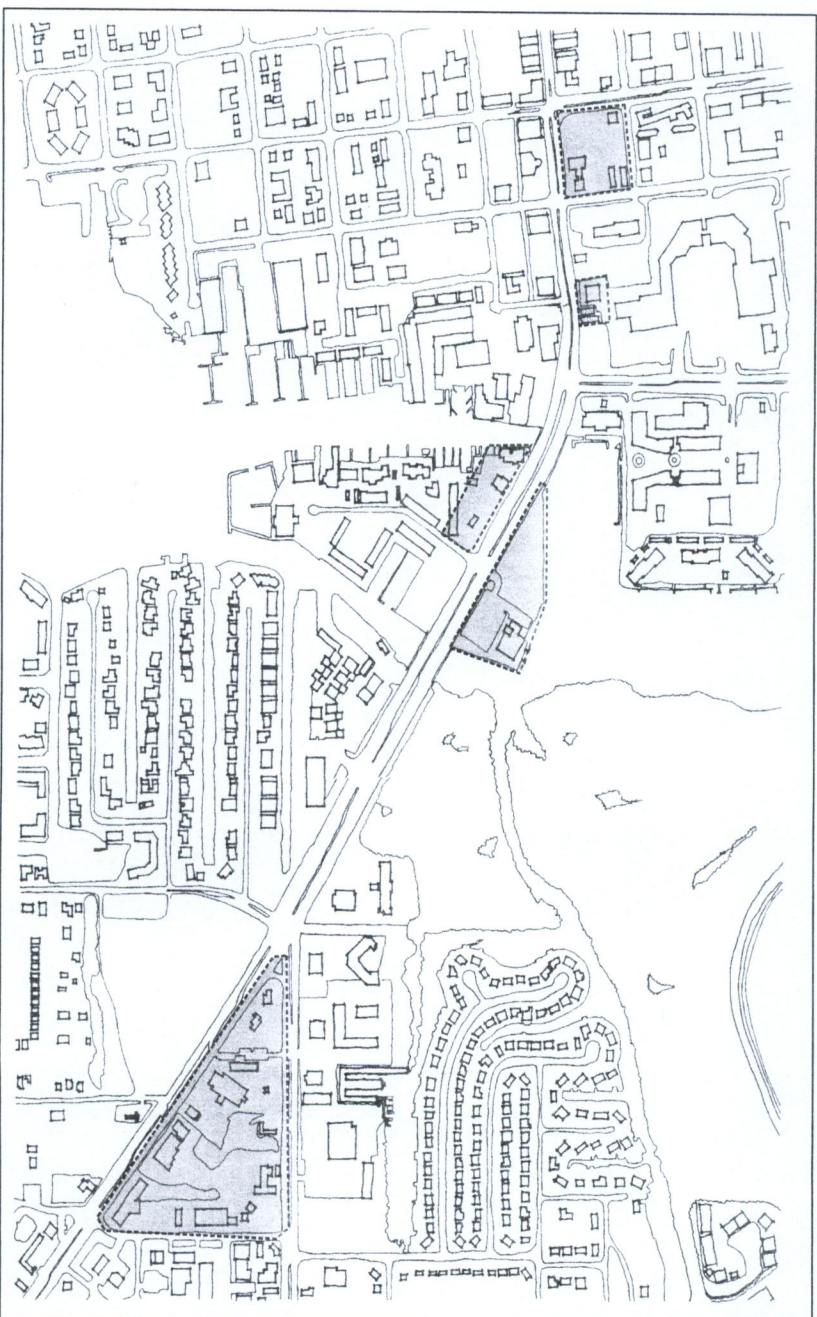


Gordon River and the canals to the east, have a high degree of visibility from the roadway. As part of a redevelopment scenario that uses the waterfront at these locations as "iconic" elements, these uses could play a major role in enhancing the visual order of the area.

The vacant parcel of land next to Joe's Crab Shack creates a major gap in the visual continuity of this part of the street. The site also enjoys scenic views to the north – up the Gordon River. However, with the restaurant as the only structure on this site, the larger parcel contributes more to a sense of suburban, rather than urban conditions. The mangrove nature preserve to the east and north provides a scenic vista from the roadway, and should be factored into any further streetscape enhancements.

The "Gateway Triangle" area is defined largely by the high volume of traffic that flows through the intersection of U.S. 41 and Davis Boulevard. At present, there is no true gateway-like feature that announces arrival into the City. The public park at the southwest corner of this intersection seems too small for this location and underutilized. Overall, this area lacks definition as a point of entry into Naples, or conversely into Collier County.

During the charrette, many participants voiced concerns about the lack of pedestrian life along this part of 5<sup>th</sup> Avenue South. These concerns stemmed directly from the rate of speed and the volume of traffic moving in and out of the City. Charrette participants were keenly aware of the need to identify possible changes within the right-of-way that could help calm traffic and create a more hospitable and safe environment for pedestrians, as well as bicyclists.

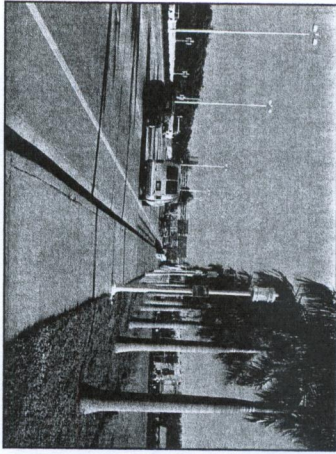


Potential Development Parcels

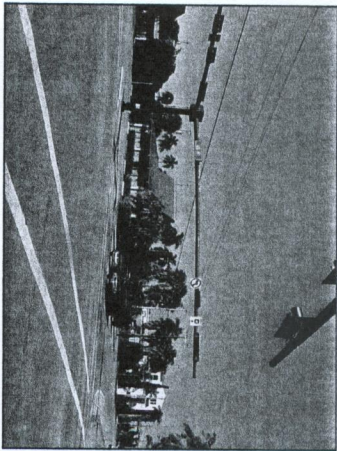




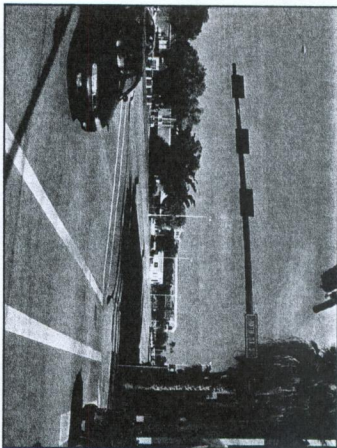
## Existing and Proposed Conditions



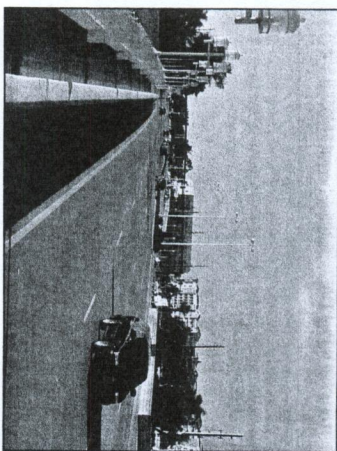
Existing: View Looking East at Riverpoint Drive



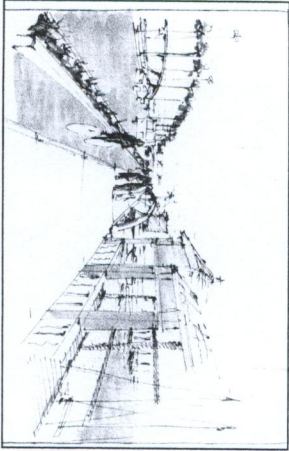
Existing: View Looking Northwest at 10th Street



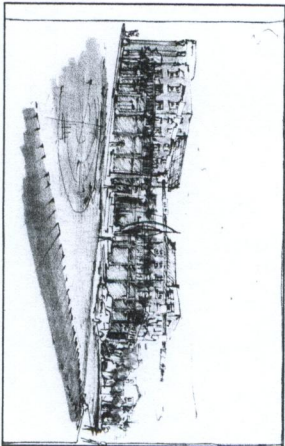
Existing: View Looking East at 10th Street



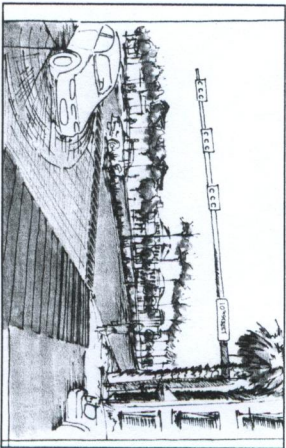
Existing: View Looking West at Gordon River Bridge



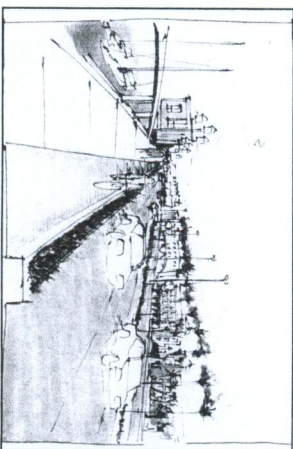
- Proposed:
- "Protected" bike lane
  - Widened sidewalk
  - Under-bridge crosswalk access



- Proposed:
- Civic Complex
  - Public Plaza
  - Public Art: Palms of Naples
  - Decorative paving & crosswalks



- Proposed:
- Decorative paving & crosswalks
  - Streetscape enhancements
  - Public Plaza



- Proposed:
- Widened sidewalk
  - "Protected" bike lane
  - Streetscape enhancements
  - Under-bridge crosswalk access



## 6.0 Sub-Areas along the Corridor

### SUB-AREAS ALONG THE CORRIDOR

For purposes of this charrette, the entire project area was divided into four sub-areas along its one-mile length. This sub-area definition helped to better understand issues related to a range of diverse circumstances, and localized conditions adjacent to each of the proposed new developments.

The sub-areas were defined as follows:

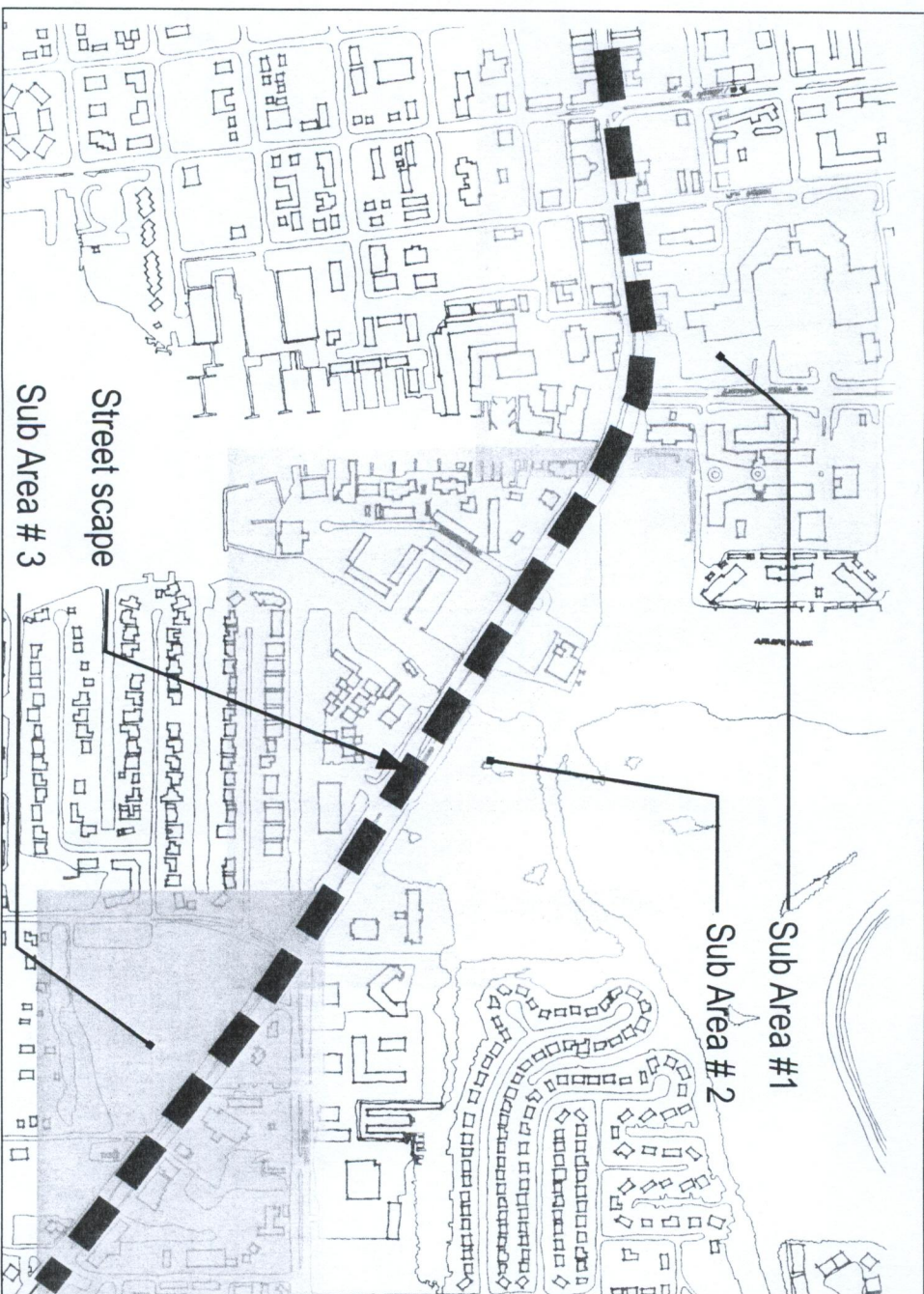
#### Sub-Area #1:

Sub-area #1 is the western-most portion of the project area. It extends from eastward from the "Shopping District" and includes the "Four Corners" intersection, a prominent vacant parcel of land at the northeast corner, the historic Naples Depot, several commercial properties, and the Tin City development. This area also includes the existing Bayfront development, and the proposed Grand Central Station project. For planning purposes, this sub-area was terminated at the west Gordon River Bridge.

The limits of this sub-area were defined as such because this was the only portion of the 5<sup>th</sup> Avenue South/U.S. 41 frontage that is substantially built out.

#### Sub-Area #2:

Sub-area #2 is defined from the western edge of "Reed Island", down to the city limits at the "Gateway Triangle". This area includes several marine-related uses, a prominent vacant parcel oriented towards the upper Gordon River, and the proposed Naples Bay Marina development. On the north side of the street, a large mangrove forest and conservation area extending up the east side of the Gordon River characterizes this area. It also has several commercial establishments, which appear to





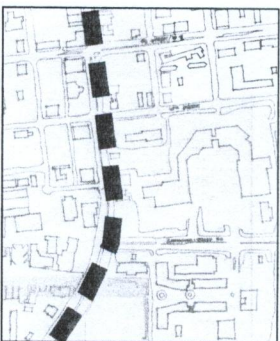
be somewhat isolated. The most recent streetscape improvements by the City and several large sign bridges over the street by D.O.T. are visible in this area as well.

#### Sub-Area #3:

Sub-area #3 is the eastern-most portion of the project area. It extends into the County and includes the proposed Ruffina development, the "Gateway Triangle" area, and the commercial area between U.S. 41 and Davis Boulevard. This area includes the existing Sandpiper Park and the adjacent residential neighborhood. The western boundary of this sub-area is Commercial Boulevard.

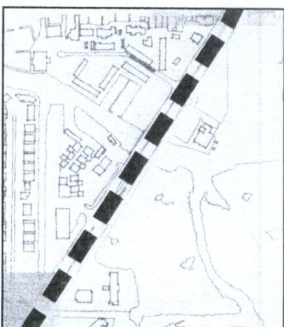
#### Sub-Area #4:

Sub-area #4 was defined as a corridor-wide focus on streetscape improvements and secondary elements. As such, this focus extends from the "Four Corners" intersection all the way to Commercial Boulevard. It also incorporated short portions of cross streets at intersections along the corridor.



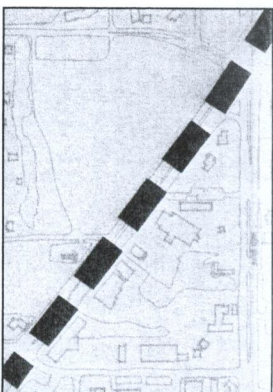
#### SUB-AREA WORKGROUP #1: ISSUES & CONCERNS

- Large volume of traffic at the "Four Corner" intersection needs to be paved down/calmed.
- Lane reductions and a traffic circle should be re-considered at the "Four Corners" intersection.
- "Four Corners" could benefit from a "grand civic gesture".
- Changes in road texture and streetscape needed to enhance the "Four Corners" intersection.
- Enhanced pedestrian crosswalks needed in the area.
- Additional public parking opportunities need to be identified outside of the Shopping District.
- Better signage needed to direct Shopping District motorists and through traffic.
- 11<sup>th</sup> and 12<sup>th</sup> Streets should be re-opened on to 5<sup>th</sup> Avenue South/U.S. 41.



#### SUB-AREA WORKGROUP #2: ISSUES & CONCERNS

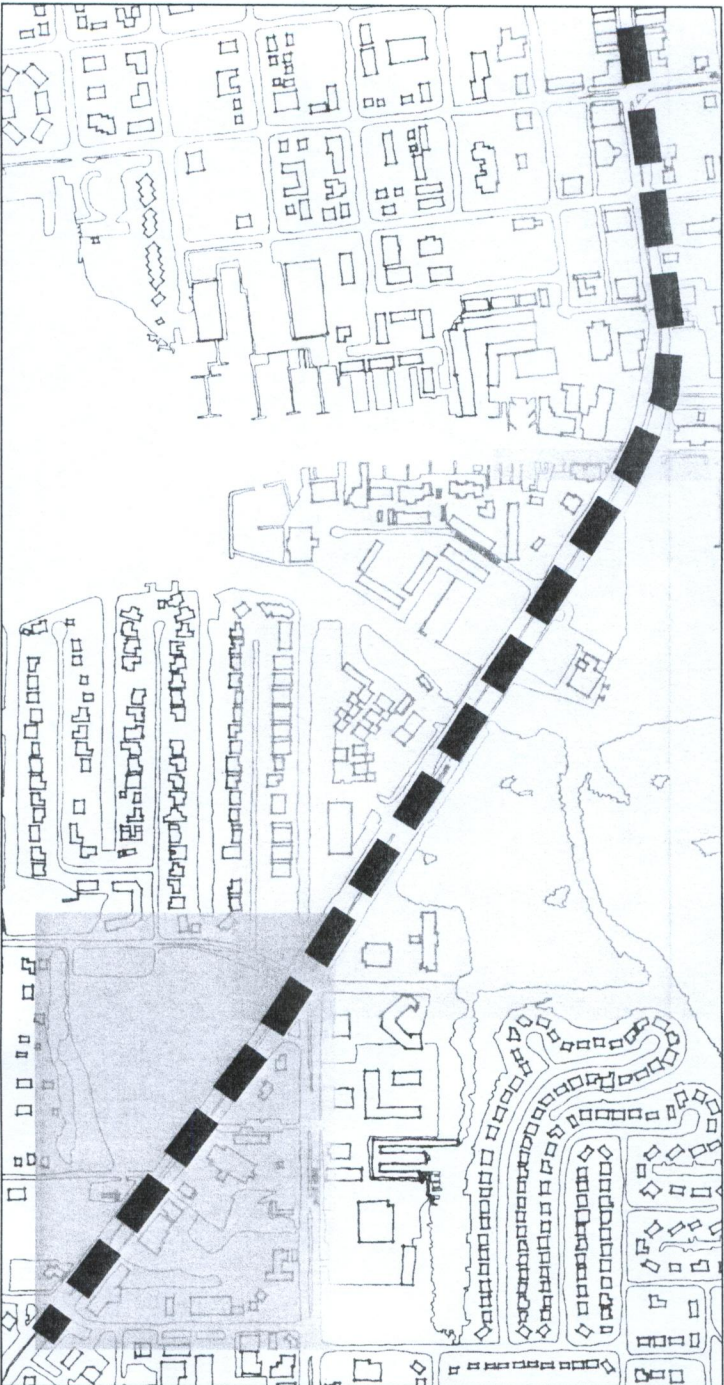
- Make crossings at U.S. 41 safer for pedestrians.
- Consider additional under-bridge pedestrian crossings to facilitate convenient movement across U.S. 41.
- Limit vehicular access to certain areas along the waterfront.
- Provide better access to the riverfront and boat docks.
- Lack of continuity in the landscape.
- Consider alternative modes of transportation along U.S. 41: water taxis, tram/trolley service.
- Establish walking trails along the waterfront for locals and tourists.
- Develop a coordinated parking plan for the entire area.
- Consider public acquisition of vacant parcel for new waterfront development.
- Waterfront view corridors should be preserved through new regulations.
- Pedestrian bridges needed to connect waterfront areas near U.S. 41.



#### SUB-AREA WORKGROUP #3: ISSUES & CONCERNS

- Entry conditions from into the County should be enhanced.
- Residential development at the proposed Ruffina project should be buffered from the traffic along U.S. 41.
- Additional crosswalks needed along U.S. 41.
- Street edges in this area need to be enhanced to help activate the public realm.
- Trolley service should be provided from this area into downtown.
- The "activity nodes" in this area should be physically connected.





#### SUB-AREA WORKGROUP #4: ISSUES & CONCERNS

- The corridor would benefit from a unified streetscape theme.
- Alternative greenway linkages should be considered.
- Water taxi service should be provided at nodes.
- A separation between motorists and pedestrians should be considered within the right-of-way.
- Rest areas and viewing stations should be provided along U.S. 41.
- Waterfront edge condition lacks definition.
- Commercial activity should be promoted along the full length of the corridor.

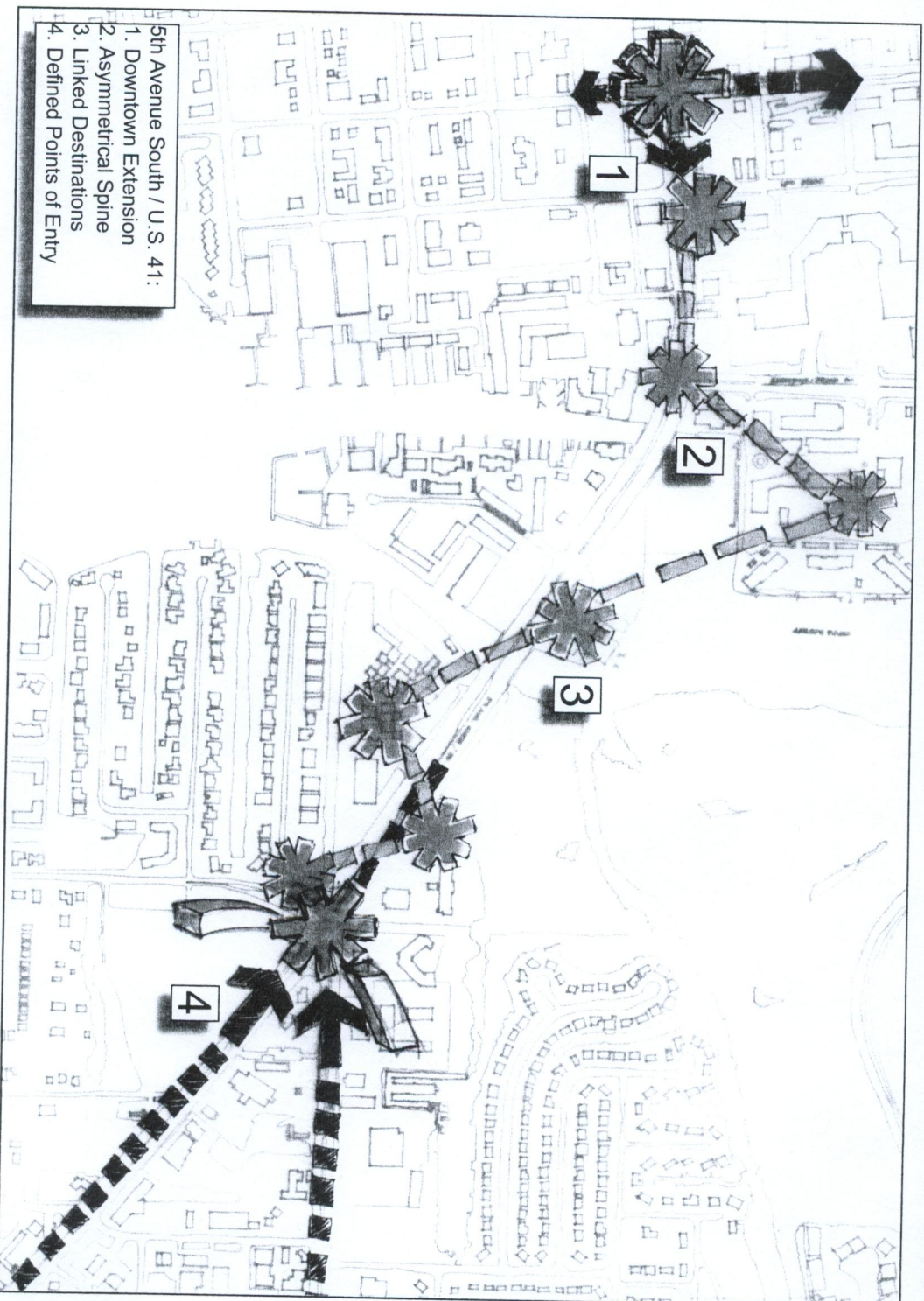


## CONCEPTUAL FRAMEWORK

The conceptual framework for the project area was based on an urban design strategy that envisions the physical structure, functional order and visual character envisioned in the future. It considers both the existing pattern of development and the configuration arising from future large and small-scale interventions along this portion of 5th Avenue South. As it extends eastward from the Shopping District, this part of 5th Avenue South is much more diverse in building placement, land usage and character. This area appears to foster activities that are more site-specific or localized, and exhibits a more irregular development pattern. Based on these circumstances, the Project Team described the project area developing as an "asymmetrical spine" in the future. That is, the existing development pattern, as well as future developments along the corridor will likely reinforce an unbalanced and discontinuous set of physical conditions from one side of the street to the other.

Although this condition is quite different from the current circumstances in the Shopping District, the urban design challenge will be to create an implied sense of building continuity and life along the street, in order to counter the adverse impacts of gaps in the physical fabric and the high traffic volume on the roadway.

Another aspect of this conceptual strategy focuses on an enhanced public realm throughout the corridor, and the creation of physical and perceived connections for pedestrians from one development or sub-area, to another one. The idea here is to identify opportunities for creating an extensive pedestrian network of sidewalks and boardwalks which connect existing destinations with new ones throughout the project area.



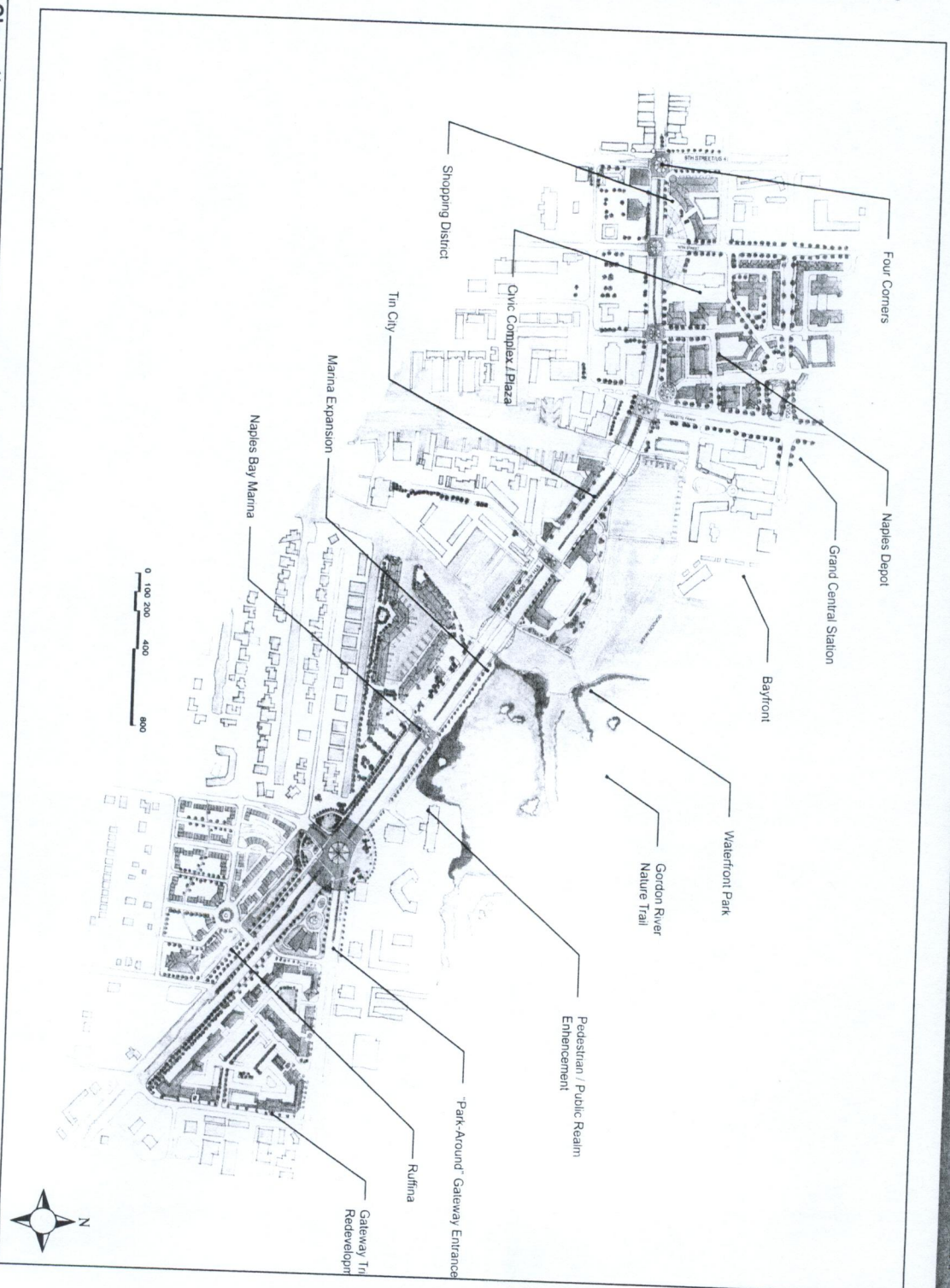


## MASTER PLAN

The master plan for the project area attempts to integrate existing and proposed projects with new infill/redevelopment opportunities, and public amenities that may materialize in the future. The plan suggests a comprehensive vision of the area as it could be developed in order to achieve the earlier stated objectives of extending the downtown and creating a memorable gateway into the City. The plan also incorporates a number of small-scale elements intended to enhance the public realm and foster pedestrian life along this part of the 5<sup>th</sup> Avenue corridor. Some these elements include: a major public plaza near the Four Corners intersection, a plaza that acknowledges the Naples Depot as an important historical feature, an entry plaza to the proposed Grand Central Station project at the corner of Goodlette-Frank Road, a large waterfront park at the northern edge of Reed Island, and a major segmented park at the City limits.

This master plan envisions creating greater public access to the waterfront and establishing connections with the Conservancy's upper Gordon River trail through a system of esplanades, pedestrian bridges and boardwalks.

Overall, this plan seeks to maximize opportunities for bolstering economic development, improving the visual order of the area, and fostering new pedestrian life that would extend from the Shopping District all the way to the eastern City limits.



## 8.0 Master Plan

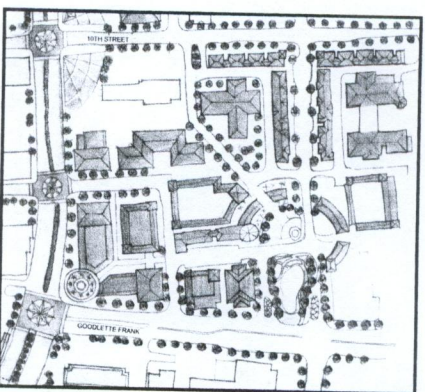


## NEW DEVELOPMENT OPPORTUNITIES

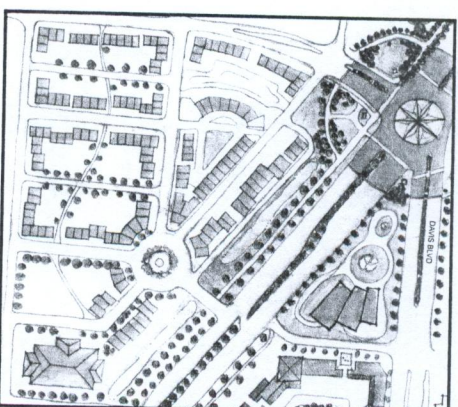
### Proposed Developments

The proposed Grand Central Station, Naples Bay Marina, and Ruffina projects will largely influence new development opportunities within the project area. It is likely that the construction of these projects will spur additional development within the project area for years to come. Although only a limited amount of vacant land currently exists within the corridor, market forces could spur redevelopment of several parcels with existing businesses. The combined effects of these circumstances and interventions by the City will greatly influence physical form of this part of Naples. If this area is to be successful as an extension of the Downtown Shopping District, all future projects will need to be looked at in holistic manner. Given the physical circumstances of this area, each new development opportunity should also be leveraged to maximize the level of urban amenities needed to ensure long-term commercial viability and design quality. This suggests that all new development achieves an appropriate level of integration with the district, rather than exist as a series of isolated projects.

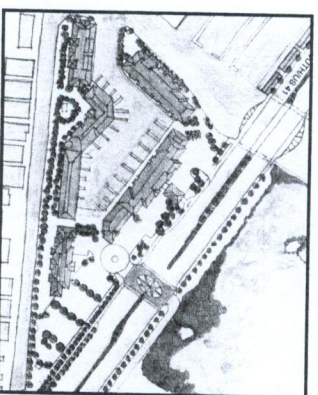
Depending on the timing of the three proposed projects, it is likely that they will influence land values throughout the corridor. In all probability, these new projects will spur redevelopment of adjacent sites, particularly where the current uses can be upgraded as a result of market forces. The most apparent opportunities can be found along the 5<sup>th</sup> Avenue South/U.S. 41 corridor on the parcel at the northeast "Four Corners" intersection, the parcels adjacent to the Naples Depot, the site occupied by Joe's Crab Shack, the marine-related uses at River Point Drive and Port O' Call Way, and the mixed commercial area in the "Gateway Triangle" area just beyond the city limits. Although they currently appear stable, the blocks between 9<sup>th</sup> and 12<sup>th</sup> Streets



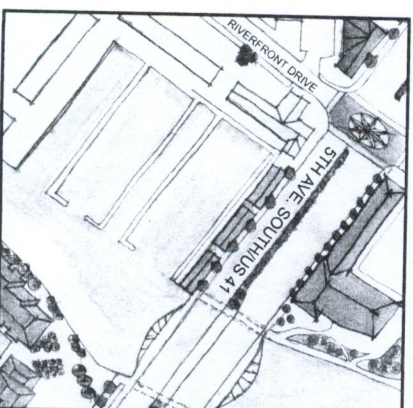
Grand Central Station



Ruffina



Naples Bay Marina



Marina Expansion



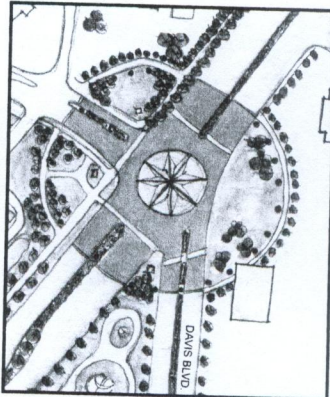
along the southern street frontage of 5<sup>th</sup> Avenue South could realize some new infill investment as well.

Many of the existing uses and vacant parcels that can be redeveloped are currently configured in small lots. In order to make these parcels more viable for development, it may be necessary to assemble them into larger land holdings that create development parcels that are commercially viable and have the ability to become activity generators. Assemblages of these smaller properties will also help to break up the piecemeal nature of the corridor as it now stands, create a cohesive urban appearance and a higher degree of functional integration with the rest of the street.

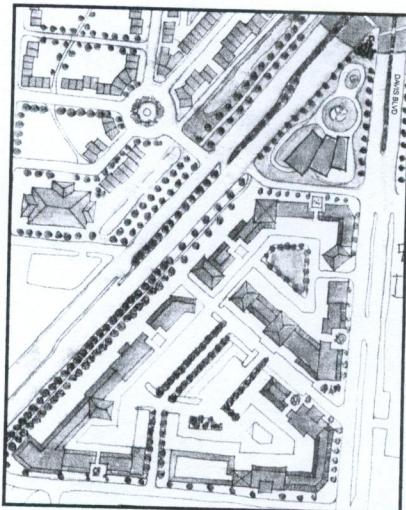
The Project Team identified several sites within the corridor that are potentially ripe for redevelopment of this type. Additionally, the Project Team also created several alternative design schemes for some of these areas. While these schemes were only developed at an initial level of design, they are intended to show the potential for building configurations, program uses and relationships with adjacent conditions. These future developments are indicated at right.

New development, whether occurring on vacant land or by the redevelopment of existing parcels, should be designed in a holistic and integrated manner in order to maximize the potential for future success. If not, individual projects may not benefit from the adjacency synergies that can be realized between different developments. All elements of each project, including landscape features, parking areas, facades, entrances, and building details should be addressed through the implementation of clear design guidelines. Proposed development guidelines for some of the major design issues are addressed in a later section of this document.

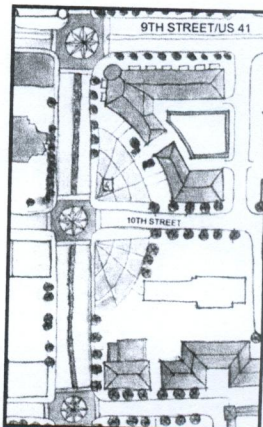
## Future Development



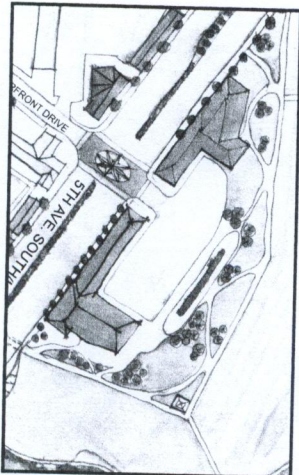
- "Park-Around"**
- Segmented Park
  - Decorative Paving
  - Entry Beacons



- Gateway Triangle**
- Mixed-Use Development
  - Workforce housing
  - Commercial Uses
  - Open Space Amenities
  - Surface Parking



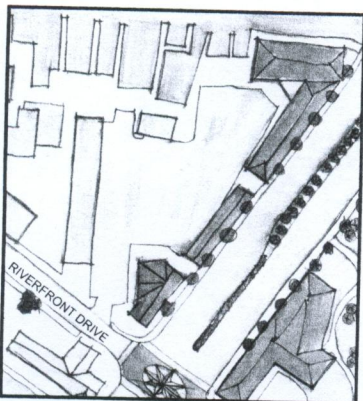
- Civic Complex**
- Mixed-Use Development
  - Civic Uses
  - Commercial Uses
  - Civic Plaza
  - Structured Parking



- Reed Island Waterfront**
- Mixed-Use Development
  - Ground Floor Restaurants
  - Waterfront Park & Esplanade
  - Surface Parking



- Nature Trail**
- Pedestrian Bridges
  - Boardwalk Trails



- Port-O'-Call Way Marine Development**
- Marine-related Commercial Uses
  - Waterfront Esplanade
  - Surface Parking

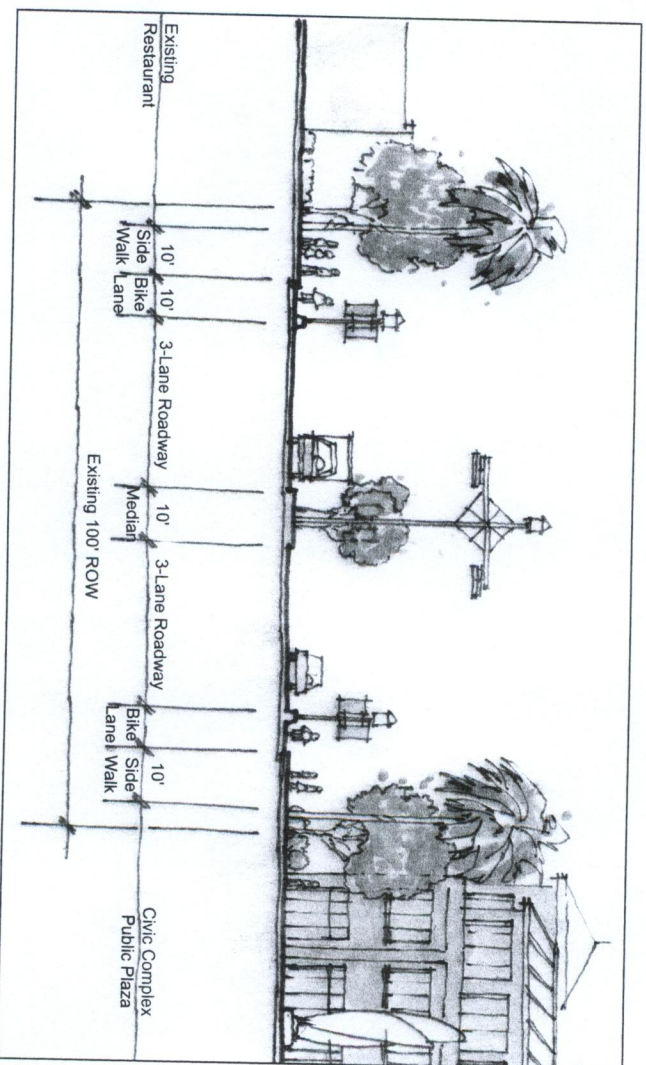


## PUBLIC REALM ENHANCEMENTS

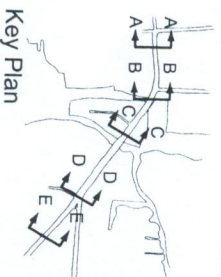
The master plan identifies a number of public realm enhancements that work in conjunction with those already in place throughout the project area. These public realm enhancements are intended to make this part of 5th Avenue South/U. S. 41 a more pedestrian-friendly environment.

The plan calls for stronger definition of pedestrian paths through widened sidewalks on both sides of the street, a series of under-bridge walkways connecting with the street, pedestrian bridges over the Gordon River, waterfront esplanades, and a boardwalk system that connects with the Conservancy trail system further north along the River. The pedestrian realm is further enhanced by new open space elements, such as the "grand civic plaza" near the "Four Corners" intersection, a corner plaza at Goodlette-Frank Road, a waterfront park, and a major gateway element at the city limits comprised of a segmented park and entry beacons.

The pedestrian realm is further enhanced through decorative crosswalks and paving patterns at each intersection throughout the corridor. These elements are intended to help calm traffic, and would be supplemented with theme street lighting, banners and public signage. The plan also calls for the relocation of the proposed "Palms of Naples" sculpture to be installed in the public plaza at the new proposed civic complex near the "Four Corners" intersection, rather than the Sandpiper Park location. Although some street planting was added to the area in the recent past, the master plan calls for a more extensive level of planting (e.g. street trees and shrubs), within and adjacent to the right-of-way.

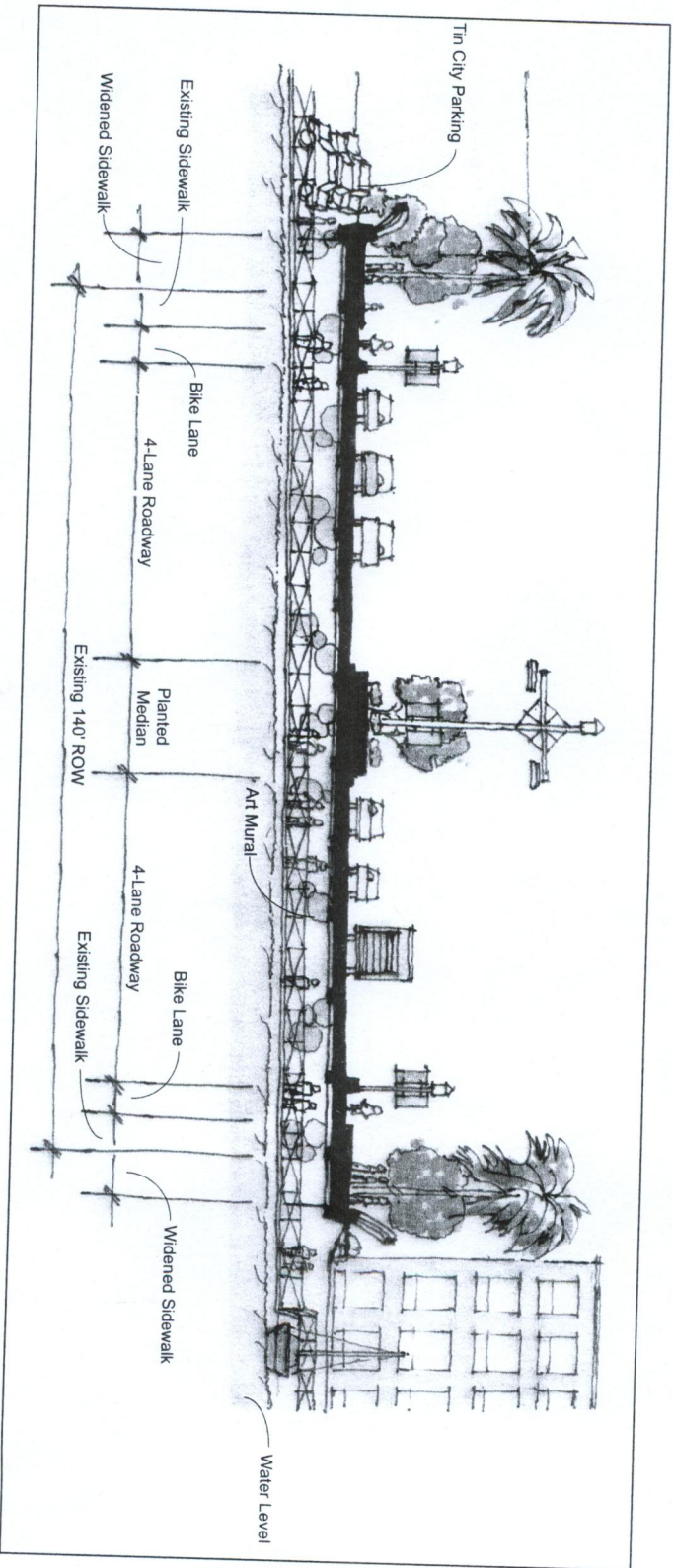


5th Avenue South / U.S. 41 Street Section A-A  
Looking West between 9th & 10th Streets

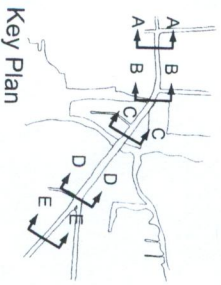


Key Plan



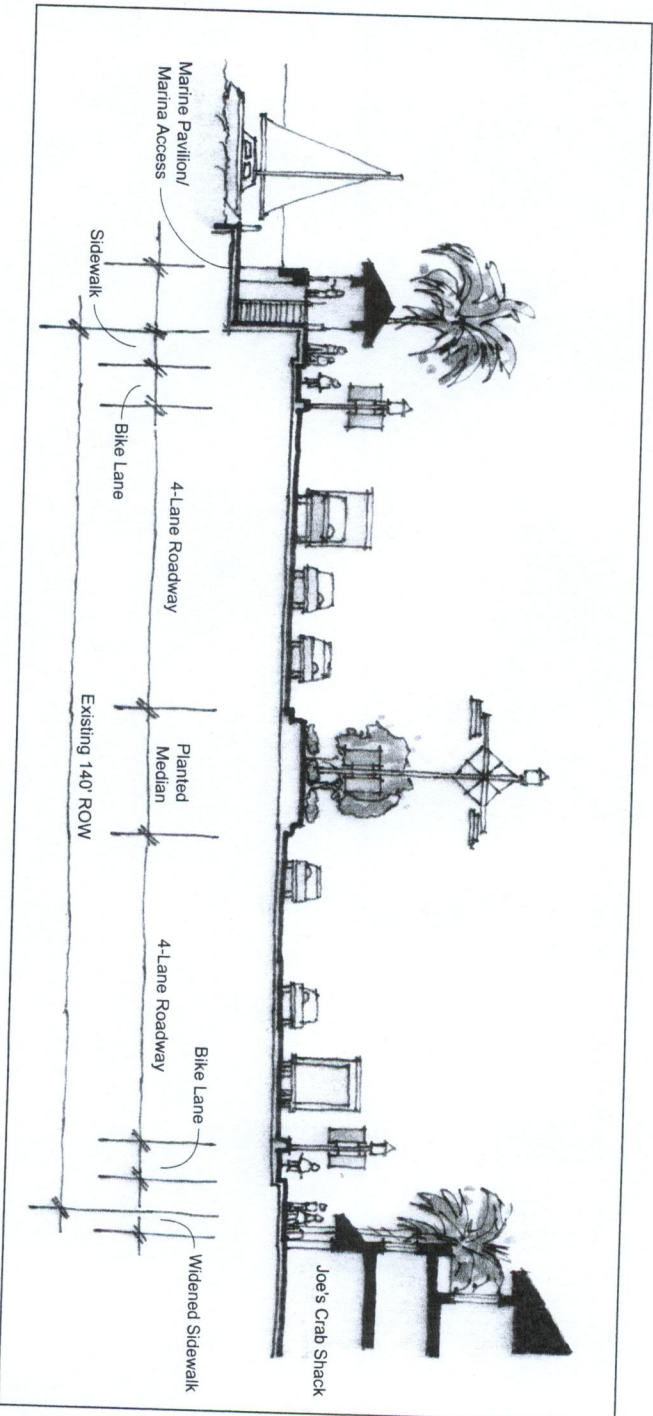


5th Avenue South / U.S. 41 Street Section B-B  
at (West) Gordon River Bridge Looking West

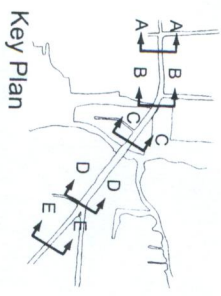


Key Plan



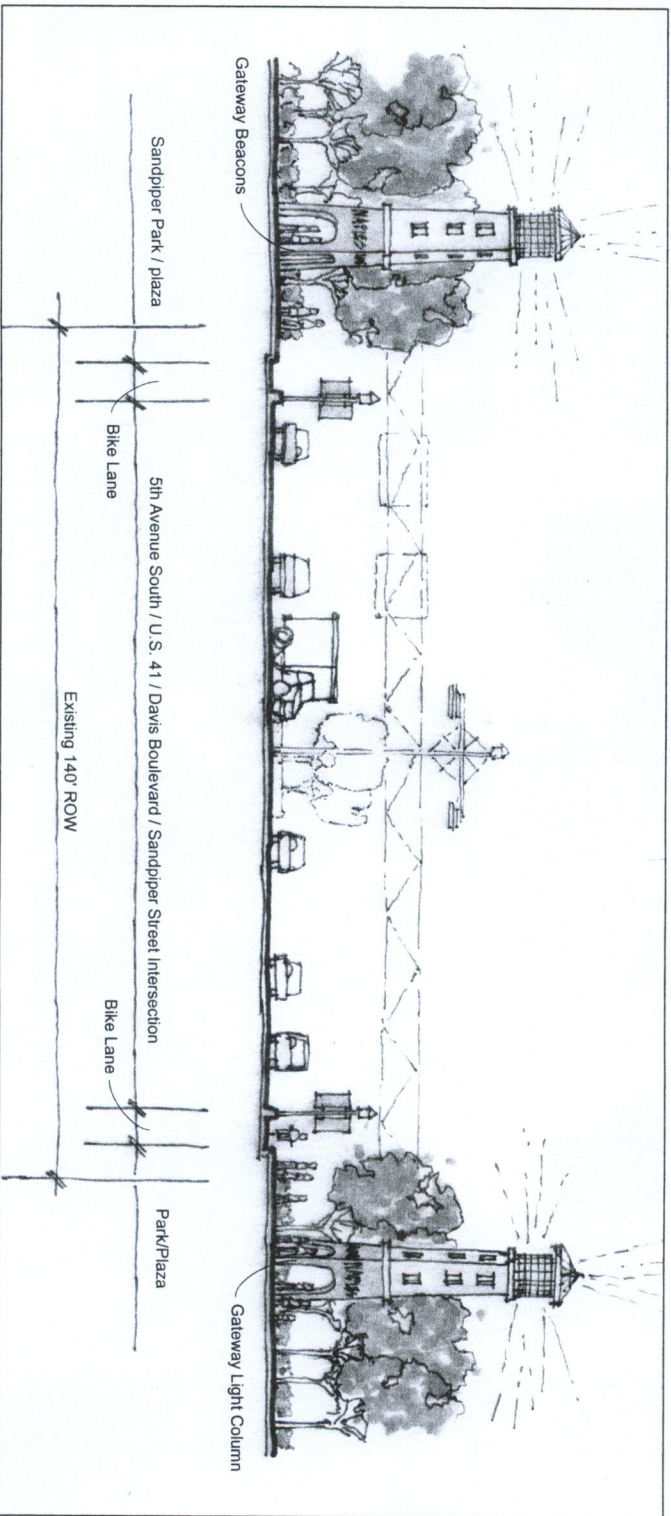


5th Avenue South / U.S. 41 Street Section C-C  
at New Marina / Joe's Crab Shack looking West

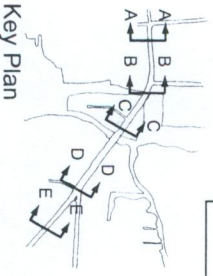


Key Plan



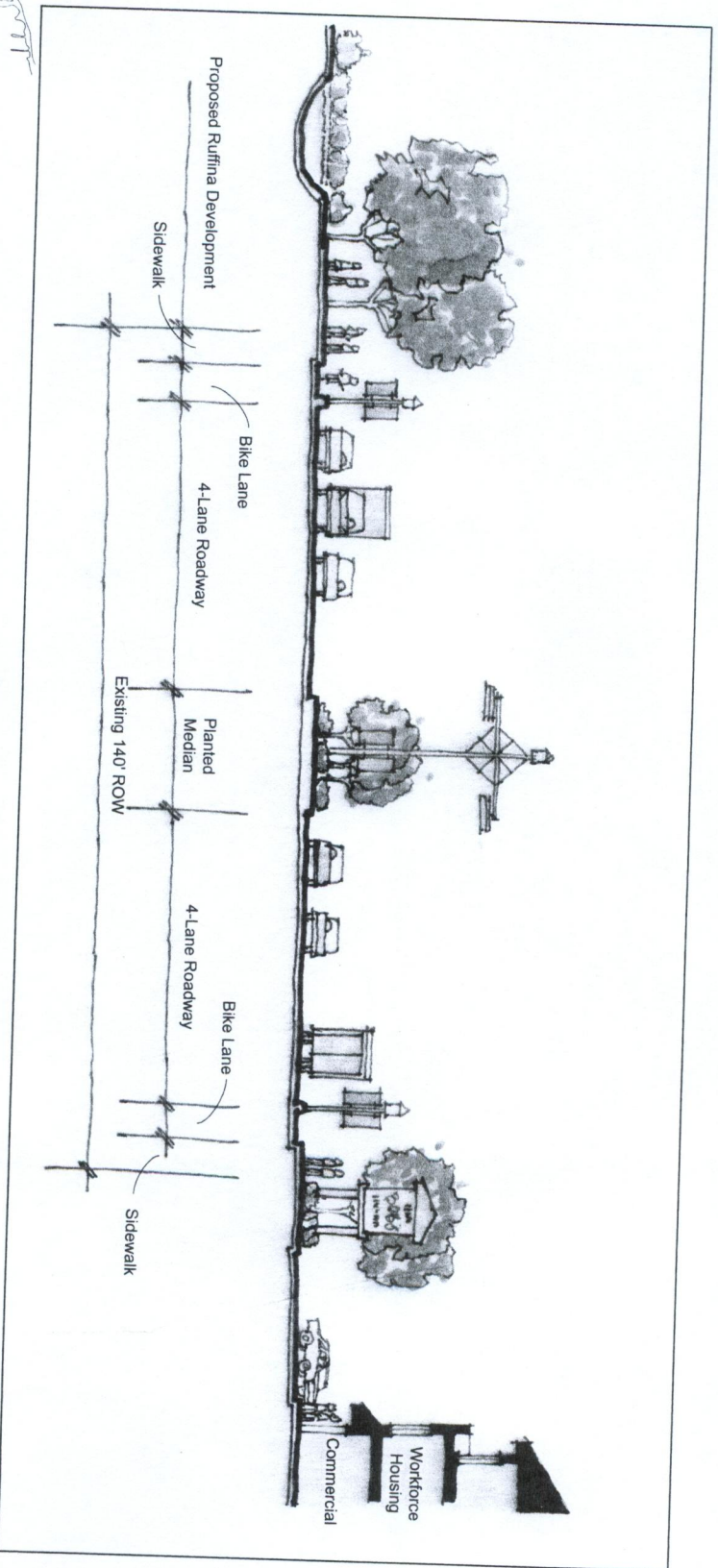


Street Section D-D at 5th Avenue South / U.S. 41 /  
Sandpiper Street Intersection looking West

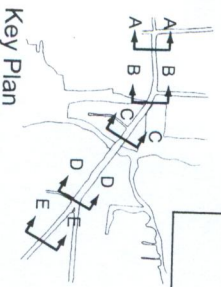


Key Plan





U.S. 41 / 5th Avenue South Section E-E at Proposed  
Ruffina Development / "Gateway" Commercial Area



Key Plan



## FOSTERING PEDESTRIAN ACTIVITY

Fifth Avenue South as it extends eastward from the Shopping District is a very active vehicular corridor with very little pedestrian life. This condition is exactly the opposite of that in the Shopping District where buildings and the roadway work in conjunction to create a well-defined pedestrian environment. Through modifications to existing buildings, new development, the addition of a variety of streetscape features, and traffic calming measures, pedestrian activity along this part of 5<sup>th</sup> Avenue South can be dramatically increased. While it may never realize the same degree of pedestrian activity of the Shopping District, the project area has the potential to accommodate a range of new pedestrian activities.

During the design charrette, several participants identified a number of methods that addressed the promotion of the pedestrian along the street and on adjacent developments. These methods included the regular placement of building entrances along sidewalks, widening sidewalks, providing resting or stopping points along the corridor, creating new pedestrian paths, intersection improvements, and improved landscaping.

### Sidewalks

Existing sidewalks along the 5<sup>th</sup> Avenue/US 41 corridor are generally 4'-5' in width. The sidewalks in most cases are located within only a few feet of the driving lanes and inhibit the feeling of safety that is needed for viable pedestrian activity. Several potential design solutions can be implemented to reduce the impact of these constraints.

First, sidewalks should be increased in width to a minimum of 8' or 10' wherever possible. This can be accomplished by adding additional sidewalk width to the edge of the right-of-way or by obtaining sidewalk easements on private property where there is inadequate right-of-way width. Sidewalk easements could be obtained during the development review process for new or redevelopment projects.

All sidewalks should be linked with those at intersecting streets to provide direct pedestrian access from adjacent areas. Additional landscaping and "hardscape" features should be installed between the travel lanes and the sidewalks in order to promote a safe walking environment. The master plan calls for a low barrier between the travel lanes and the bicycle lane, with small shrubs to create a physical separation between the pedestrian/bike realm and the roadway. This feature could be easily added to the existing right-of-way without sacrificing travel lanes, and would provide an added sense of safety while simultaneously enhancing roadway aesthetics.

Next, spaces for rest or observation should be created periodically along the corridor, especially at the two bridges over the Gordon River. These types of spaces would allow pedestrians to take a break from their trip and enjoy scenic views up and down the River. The areas could further be outfitted with shading devices for sun protection and serve as bus stop locations for a future transit system.

### Boardwalks, Esplanades and Pedestrian Bridges

The master plan also envisions several new pedestrian elements throughout the project area. In an effort to provide

greater access to the waterfront, the plan calls for an esplanade system at the northern edge of "Reed Island". This system would connect with a series of boardwalks at the edge of the mangrove forest and provide direct access to the Gordon River nature trail to the north. In addition to these, a number of pedestrian bridges over the narrowest portions of the River are also proposed. These features would provide additional connections to the Bayfront project as well.

### Under-Bridge Crosswalks

A central factor in promoting pedestrian traffic for future development within the 5<sup>th</sup> Avenue South/US 41 corridor will be how to easily move pedestrians from one side of the road to the other. Currently, the at-grade crosswalks at each intersection are somewhat dangerous due to the distance to opposite side of the street, the speed of vehicular traffic and the phasing of traffic signals. Also, long stretches occur between intersections in portions of the corridor, partly due to the two existing bridges. Based on input from charrette participants, the Project Team has identified several features to help move pedestrian traffic back and forth across 5<sup>th</sup> Avenue South/US 41.

One of these features is the addition of three other under-bridge connections – similar to the one that currently exists at Tin City. These connections could be constructed in front of the existing bridge abutments, and along with a series of access points from the sidewalks at street-level, would allow for convenient pedestrian access to the Bayfront development and the Gordon River trail system to the north. Field investigations by the Project Team suggest that there is sufficient headroom clearance for constructing these walkways. Although the existing crosswalk provides a convenient and even interesting means to cross US 41, it has a stark appearance and no identification at street level to let pedestrians know it exists.

Both the existing and future under-bridge crosswalks should be clearly identified at the street level through the construction of an entrance feature and signage. They should provide a system of ramps to accommodate pedestrians with disabilities and bicyclists as well. Additionally, these crosswalks should be well lit, and their walls should be accented with public artwork. This can be accomplished with the assistance of local artists. Other features that these crosswalks should include are the required safety features and pedestrian amenities.

In addition to sidewalk enhancements, each intersection and its associated crosswalks along the corridor should receive decorative or ornamental paving treatments. These treatments serve a number of functions, which include providing a break in the driving surface to let alert motorists that they are moving through a pedestrian zone and providing a regular visual amenity that differentiates prominent intersection conditions.

5th Avenue South / U.S. 41 Urban Design Charrette Naples, FL	City of Naples Planning Department Planning Advisory Board	University of South Florida School of Architecture and Community Design	24
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## TRAFFIC AND PARKING

As mentioned earlier, this portion of 5<sup>th</sup> Avenue South/US 41 functions as a high-volume arterial and has inherent conflicts with attempts to make it a pedestrian-friendly environment. While the master plan does not envision the loss of any traffic lanes from Goodlette-Frank to the city limits, it does identify several measures that are intended to mitigate the impact of local traffic. These measures will be crucial in helping to create a "sense of place" and establishing conditions that are conducive to pedestrian life. Currently, traffic through the corridor moves around 40 mph; this speed will need to be reduced so that pedestrians feel safer walking along the road and crossing the road.

During the design charrette, a significant amount of discussion was devoted to measures that would lighten some of the traffic volume along the corridor. The preferred method of handling this was to re-route a portion of US 41 at Goodlette-Frank Road, north and then back over to 9<sup>th</sup> Street further north. This "Bypass" would take through-traffic primarily serve local traffic. As such, the entire "Four Corners" intersection could be transformed to conditions closely resembling those in the adjacent Shopping District. This would also allow for a reduction of traffic lanes for three full blocks and improved pedestrian access to the proposed civic complex and plaza. Re-routing traffic in this manner would also provide greater visibility and vehicular access for the proposed Grand Central Station project.

### Intersections

The treatment of intersections is also a main design consideration within the project area. As mentioned in the Pedestrian Activity section, the treatment of intersections helps drivers become aware of pedestrians through changes in the color, texture and pattern of the driving surface. This change in driving surface has proven to be an effective traffic-calming device to slow driving speeds through intersections. Treatments at intersections can also be designed as a unifying element throughout the corridor. Street corners at these intersections can become major design features through the coordination of crosswalks, signage, traffic signals, and landscape elements.

As new developments come on line, the City, along with the D.O.T., will reevaluate the design speed of the roadway, as well as signal phasing and safety features at each intersection along the corridor. Without question, these new developments will add to the volume of traffic in the area, as a greater number of vehicles will be seeking access to and from this section of 5<sup>th</sup> Avenue South.

### Parking

Parking within the 5<sup>th</sup> Avenue South Shopping District is provided primarily at the municipal parking garage, and through a series of on street, back of the block surface lots and adjacent street provisions. While on-street parking is not feasible in the project area due to the functional classification of this roadway, other measures will have to be implemented in order to provide adequate parking for future developments.

The master plan calls for the construction of a new municipal parking structure on 4<sup>th</sup> Avenue South, between 9<sup>th</sup> and 10<sup>th</sup> Streets. This structure would be part of the envisioned civic complex at the northeast portion of the "Four

Corners" intersection. Because of the limitations of available development parcels, no other public parking is envisioned. All parking for each new development should be adequate for each project.

The location of parking lots should be either adjacent to or behind the buildings, except for taxi stands and drop-off areas. As noted previously, parking lots located between building entrances and sidewalks will discourage pedestrian traffic. Wherever feasible, curb cuts for parking entrances to US 41 or side streets should be shared in order to reduce curb cuts through the sidewalk. Similarly, the use of parking agreements or easements should be encouraged so that uses operating at different peak times throughout the day can share parking. Additionally, parking areas that are not hidden from view by the building facade should be placed behind a street wall or visual landscape screen at the sidewalk edge. This technique will allow the corridor to maintain a unified street edge along the sidewalk while also providing a more aesthetically pleasing appearance to passing motorists and pedestrians.

Large surface parking areas should be broken up into smaller components to reduce the effect of large paved expanses and to provide a visual amenity. Rows of parking spaces should not exceed 10 spaces without being interrupted by a landscaped island. Landscaped islands which include at least one tree should be located at the ends of parking aisles to clearly define parking areas.

Additionally, a clear pedestrian path should be provided from parking areas to building entrances. This path can be created through a combination of alternate paving treatments and landscaping. Pedestrian paths can also be used to separate smaller components of large parking areas. A courtyard at the rear pedestrian entrance to the building can be used to transition the parking area space from the building while also serving as an aesthetic feature on the rear facade. Similarly, a clear pedestrian path from the sidewalk along US 41 should be provided to parking areas to encourage people to park their cars and walk to other areas along the corridor.

Lighting within parking areas is also an important component of the overall site design. Lighting should be adequate to ensure safety but should not dominate the parking area with excessive brightness or excessive fixture heights. The appearance and scale of lighting poles and fixtures should also be coordinated with the rest of the building's design features.

Parking garages that are incorporated within private developments must also be designed to continue the street edge where possible. In areas where developable land lies below the street grade, garages can be used as the ground floor. However, where garages are placed at grade with US 41, their facades should be treated to the same level of architectural detail as other buildings and should be placed along the sidewalk edge.

## 12.0 Traffic and Parking



## GUIDELINES

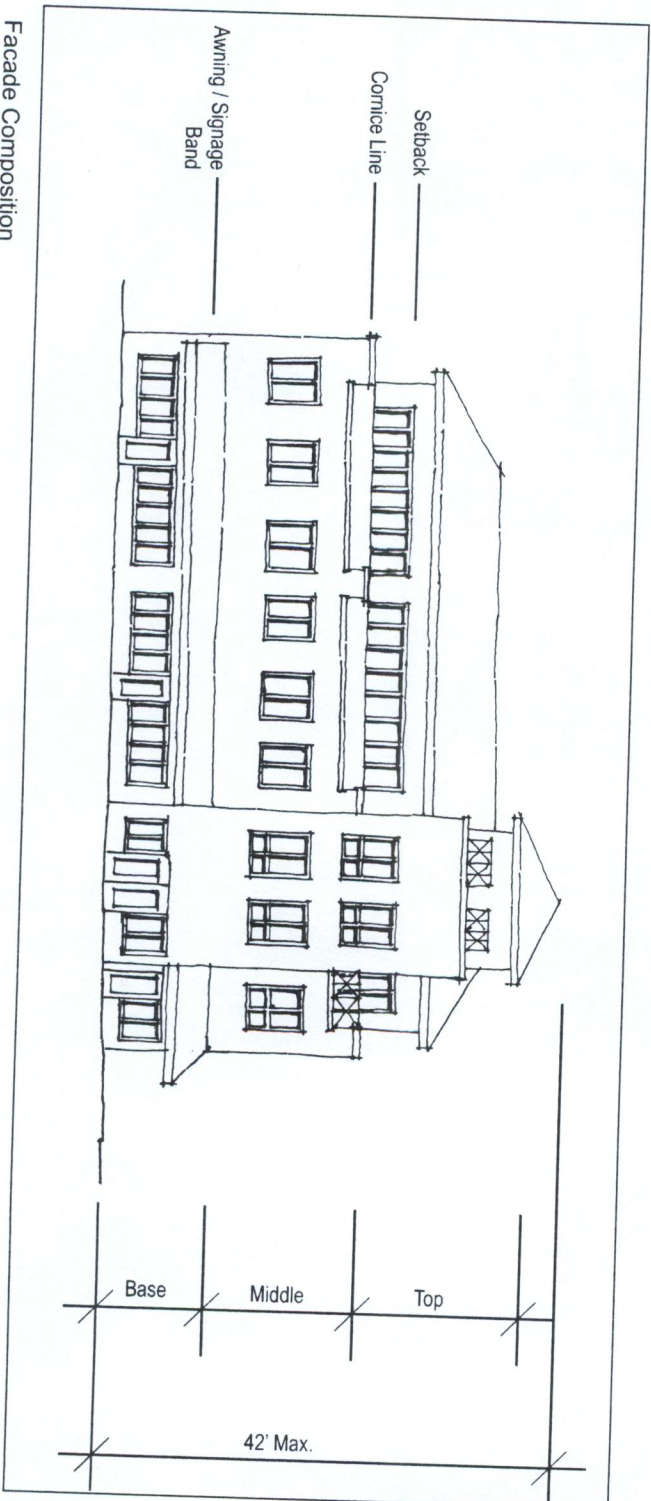
As this part of the 5<sup>th</sup> Avenue South corridor develops over time, it is imperative that the City implements a set of design standards that will ensure orderly growth and visual character that is consistent with the Shopping District. As a regional arterial, the corridor already exhibits characteristics that are typical of most commercial strips. However, if this area is to serve as an extension of Downtown, it must develop with the same level of visual and functional order that is already established.

The following are a set of general design guidelines that will help accomplish this goal. At this point, these guidelines are intended to be "performance based" recommendations. Should the City seek to have these adopted as part of the land development regulations for the corridor, they will need to be expanded as more "prescriptive" measures.

### Facade Composition

Given the diverse character of buildings from one block to the next along this portion of 5<sup>th</sup> Avenue South, it is important that new projects present well-composed facades to the street. New buildings should achieve the same level of architectural distinction established by those in the Shopping District. The massing of individual buildings should contribute to clearly discernible "street wall" conditions when viewed in relationship to adjacent structures. This condition will help establish a sense of visual order and will create implied connections with the Shopping District.

All new buildings should have clearly defined cornice lines at their uppermost levels, rather than flat parapets. This architectural feature will help reinforce a sense of visual order and help unify the



Facade Composition



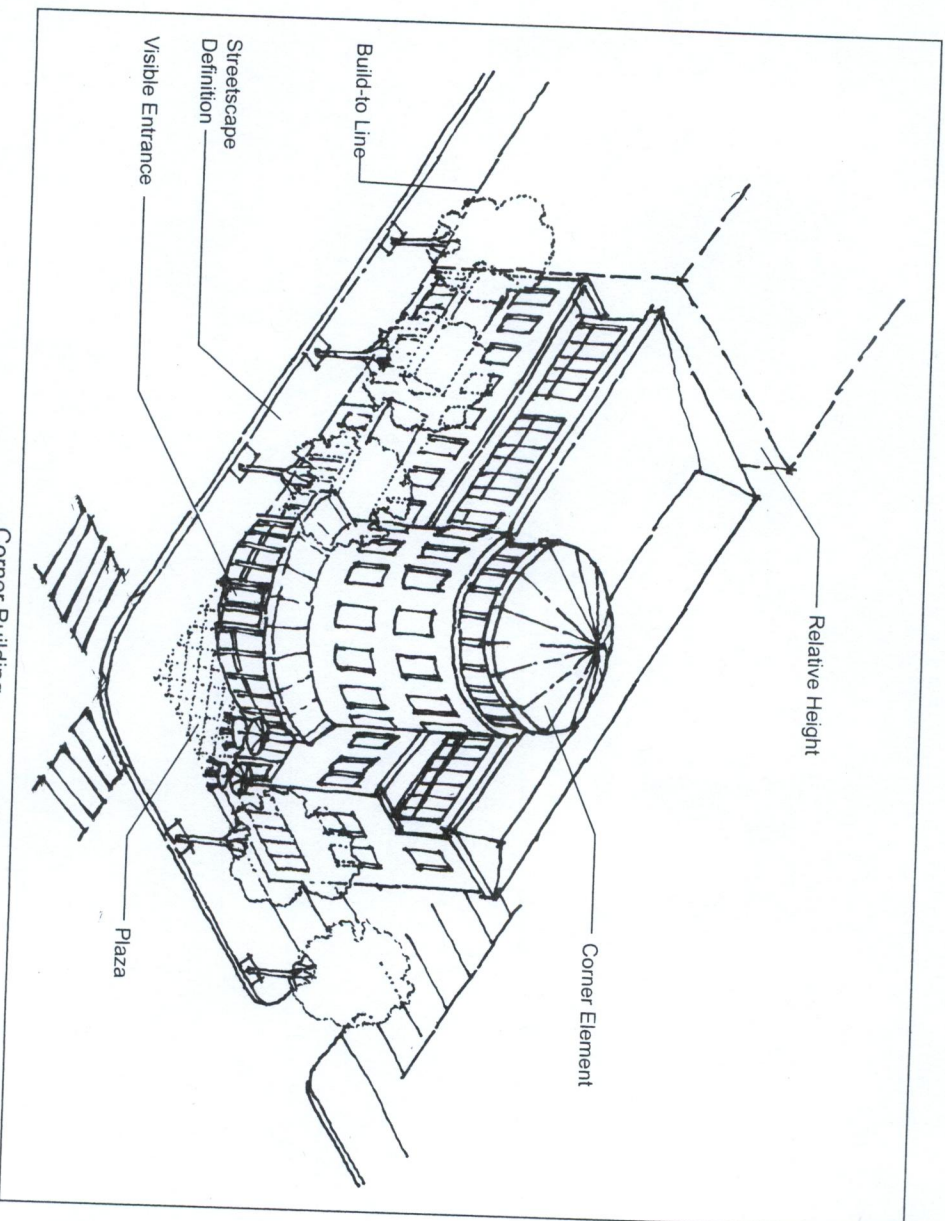
general appearance of the corridor. New buildings should also use "punched opening" fenestration patterns in street facades, with vertical window inserts. No "strip windows" or band windows should be allowed. Recesses in building facades should be for either massing relief or where ground-level open spaces are incorporated. The height of all new buildings should be consistent with the 42' limit prescribed by existing zoning. Colors and materials for new buildings along this part of 5<sup>th</sup> Avenue South should be consistent with those in the Shopping District.

Awnings and canopies over sidewalks should be encouraged throughout this area. These elements should be incorporated in new buildings where they will serve as pedestrian realm and architectural enhancements. All signage should be restricted to a horizontal "signage band" above storefront windows. No pylons or pole-mounted signs should be allowed at the edge of the right-of-way.

### Building Placement, Scale & Prominent Visual Elements

Perhaps the most important component to creating a pedestrian-friendly environment is building placement relative to the street, the massing proportions of the structure, and the prominent physical elements that suggest entry and convenience. Similar to the placement of buildings in the Shopping District, new buildings in the project area should be positioned close to the right-of-way, to better help define streetwall conditions. Buildings separated from the sidewalk space by parking lots generally do not encourage pedestrian traffic.

Although buildings in the project area are limited to 42 feet in height, each new structure should be designed to create a clearly discernible base that relates to the public realm. This can be accomplished





through a continuous storefront condition at the ground level or through setbacks at the top-most level. Furthermore, new buildings should include features that are easily understood by the general public as access, protection and convenience. This includes such features as awnings, arcades, colonnades, courtyards or plazas, and other similar design elements that provide interesting spaces for pedestrians to gather and interact adjacent to buildings. Spaces such as these can also be used for outdoor entertainment, or "sidewalk style" cafe dining. The use of awnings, arcades or a continuous colonnade would also provide protection from the sun and rain. Only buildings that provide these features should be allowed to break the streetwall.

### Corner Buildings

Buildings located at corners or which have corner-like conditions have particular significance along the street edge, since they establish the order for the entire block and often serve as visual landmarks at intersections. Facades of corner buildings should be brought to the sidewalk line along both streets or recessed to allow for some other prominent feature. A prominent vertical element should be included at building corners, such as a tower, turret or an extended floor height. Both facades of corner buildings should be architecturally contiguous and treated with the same level detailing. This will help promote continuity and allow for an appropriate facade termination. Finally, building entrances should either be provided on each street facade or through a common corner entrance so that pedestrians have a clearly defined point of access from sidewalks.

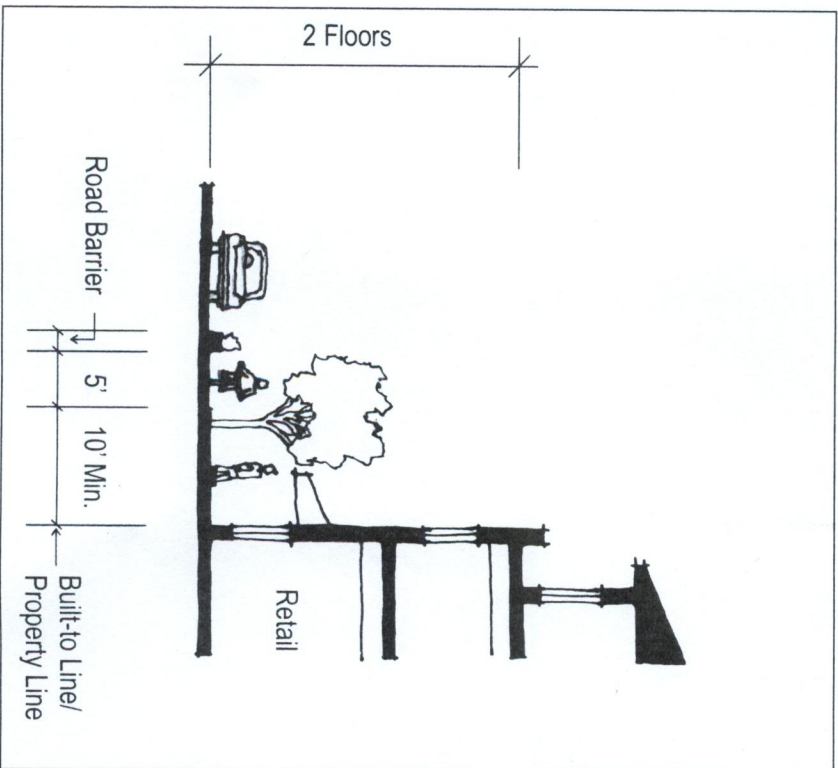
The scale of building elements is also important to developing a pedestrian-friendly environment. First floor levels should be placed at grade with the sidewalk so that pedestrians can clearly see entrances, store signage, and window displays. At least one primary entrance should be located on the street facade of each building. The height of windows and doors should be developed at a human scale to create a comfortable pedestrian environment. Cornices, a change in materials, material scale, or color should be used on building facades where the transition of the first floor to additional floors occurs. This will contribute to developing the proper scale for pedestrians and provide visual interest in the buildings. The design guidelines section of this document provides specific recommendations on the placement, scale, and proportion of facade treatments.

### Median and Street Landscaping

The existing median landscaping in the project area is too small in scale relative to the width of the roadway. Additionally, the existing planters and other hardscape features, such as street lighting, do not have a coordinated appearance and are not appropriate for the ultimate design intent of the project area.

Landscaping within the median and along the sidewalks should be coordinated to separate pedestrians from motor traffic, provide shade and enhance the aesthetic appearance of the study area. Street trees in the medians should be large enough to create a notable presence within the wider right-of-way of the project area. Similarly, trees along the sidewalk areas can be planted to help unify the different segments of the corridor and provide shade areas for

pedestrians to rest. Appropriate landscaping along the sidewalks will also serve as a buffer between pedestrian and vehicular traffic, which will enhance the feeling of safety for those walking. Median landscaping can also be designed to diminish the expansive feeling of the multiple traffic lanes by visually separating the east and westbound traffic.





## Service Areas

The treatment of service areas must also be considered during the site design process for new or redevelopment projects. Trash dumpsters, mechanical equipment and loading areas must be adequately designed so that they are aesthetically pleasing and do not disrupt pedestrian traffic flow between buildings or parking areas. Screen walls around these areas should be treated with the same level of architectural detailing as the principal structure and landscaping should also be incorporated where possible.

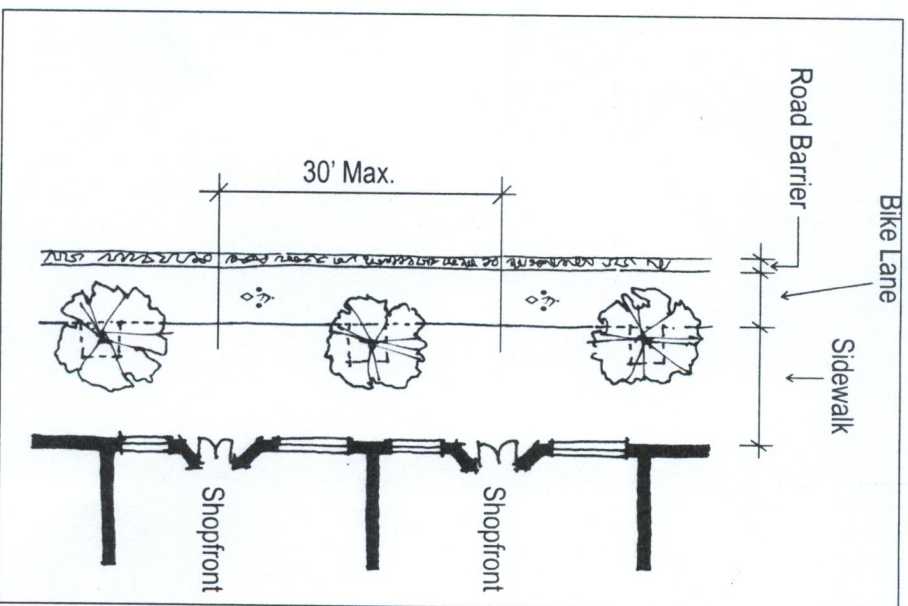
## Pedestrian Amenities

Familiar pedestrian amenities, such as plazas, arcades and courtyards, should be encouraged. These public spaces should be furnished with features such as overhead protection, seating areas, public art and water features. Where new projects are directly adjacent to the waterfront, they should provide continuous public access through the incorporation of walkways and esplanades. In addition to these, each new project should provide a level of landscaping and materials that meets established standards, and provides the necessary visual enhancements needed for the general project environment.

## Building Entrances/Shopfronts

In order to promote pedestrian activity along the entire length of the 5th Avenue / U.S. 41 corridor, building entrances should be encouraged as often as possible. The frequency and spacing of building entrances will allow for greater opportunities for pedestrians to interact with ground floor uses and alleviate dead zones along the street. Building entrances should be spaced no more than 30 feet from each other.

All buildings should provide shopfront conditions at the ground floor, similar to buildings in the shopping district. Shopfronts should incorporate ample (not excessive) glazed areas that are well proportioned with the fenestration for other parts of the building. Shopfront windows should incorporate glazed areas in four-foot-wide panels. Blank portions of shopfronts should be limited to no more than ten feet.



Building Entrance Spacing



## 14.0 Recommendations and Priorities

### RECOMMENDATIONS AND PRIORITIES

In order to address the issues raised during the charrette and realize the objectives that were established, the Project Team makes the following recommendations. It is further suggested that these items be addressed as priorities relative to the future of the 5<sup>th</sup> Avenue South/U.S. 41 corridor:

#### Development

Although there are only a few vacant parcels within this part of the 5<sup>th</sup> Avenue South/U.S. 41 corridor, the City should encourage talks between property owners and the development community. The City should work with existing property owners to see that each parcel is developed in a manner that allows for private objectives to be realized, while simultaneously realizing broader public objectives as well.

The property at the northeastern portion of the "Four Corners" intersection will be crucial in linking the corridor to the east with the Shopping District. The City should continue efforts to acquire this land as a "strategic development parcel" in the revitalization of the corridor. The City should also work with other property owners with existing buildings that are not of the architectural quality envisioned for the future of this area. Older structures with viable business uses should be renovated to the same standards as new developments along the rest of the corridor.

The City should work with the Naples Depot and the Collier County Museum System to see that the Depot structure is enhanced as a cultural facility that is truly important to the existence and evolution of Naples. This site should also have some visible physical, rather than name only, relationship to the proposed Grand Central Station project just to the north.

The City should continue to evaluate each proposed development to seek refinements that will ensure the best outcome relative to the district as a whole.

#### Public Realm

Given the physical circumstances that currently exist in this area and the challenges in transforming it to a more pedestrian friendly environment, the City should continue to hold public meetings with local residents and business/property owners. A concerted effort should be made to get all affected property owners to "buy into" the benefits of creating better public access and connectivity to the waterfront. In addition to this, the City should require each new development to include certain public amenities and should establish standards for open space, streetscape and other pedestrian amenities.

The City should partner with the Nature Conservancy of Southwest Florida to help realize its Gordon River trail system. Linking this system with the 5<sup>th</sup> Avenue South/U.S. 41 corridor would prove to be a significant benefit in helping to populate the area and make it more inviting for pedestrians.

The City should seek to establish a more visually pleasing and memorable set of entrance conditions at the eastern end of the corridor. At least one additional portion of the gateway condition suggested in this document could be

realized with further negotiations with one existing property owner within the city limits. Other portions could be realized after the Ruffina project is developed.

#### Regulatory

The City should create (or expand the existing) zoning overlay district for this portion of the 5<sup>th</sup> Avenue South/U.S. 41 corridor. This will allow for the refinement of zoning and land development regulations that are needed to ensure a cohesive development pattern evolves for this area, and that certain public objectives are realized. The City should also create a graphic code to accompany changes in regulations pertaining to this area.

#### Traffic & Right-of-Way Improvements

The City should consult with the Florida Department of Transportation to explore the possibilities of diverting some traffic off of 5<sup>th</sup> Avenue South, from 9<sup>th</sup> Street to Goodlette-Frank Road – thereby creating a U.S. 41 Bypass. If this proves feasible, it would allow for the redesign of the "Four Corners" intersection and the possibility of reducing the roadway in this area.

The City should also consult with D.O.T. regarding additional streetscape (e.g. landscape plantings, unified or themed signage, light standards, banners, etc.) and pedestrian enhancements along the corridor and the creation of a protected bicycle lane and wider sidewalks.

The anticipated level of development throughout the 5<sup>th</sup> Avenue South/U.S. 41 corridor will also create the need for some mode of public transportation in the future. To gain better insight on the increases in traffic associated with future development, and to identify opportunities for alternative means of access to the Shopping District, the City and the County should conduct a traffic study, or more comprehensive transportation demand analysis. In addition to this, the City should initiate discussions with Collier County regarding the implementation of a bus or trolley system along the corridor, as well as a water taxi system connecting points along the Gordon River, Naples Bay and other coastal locations along the Gulf. Given the anticipated level of development and other commercial activity along the 5<sup>th</sup> Avenue/U.S. 41 Corridor, a transit system in this area would benefit both the City and the County.